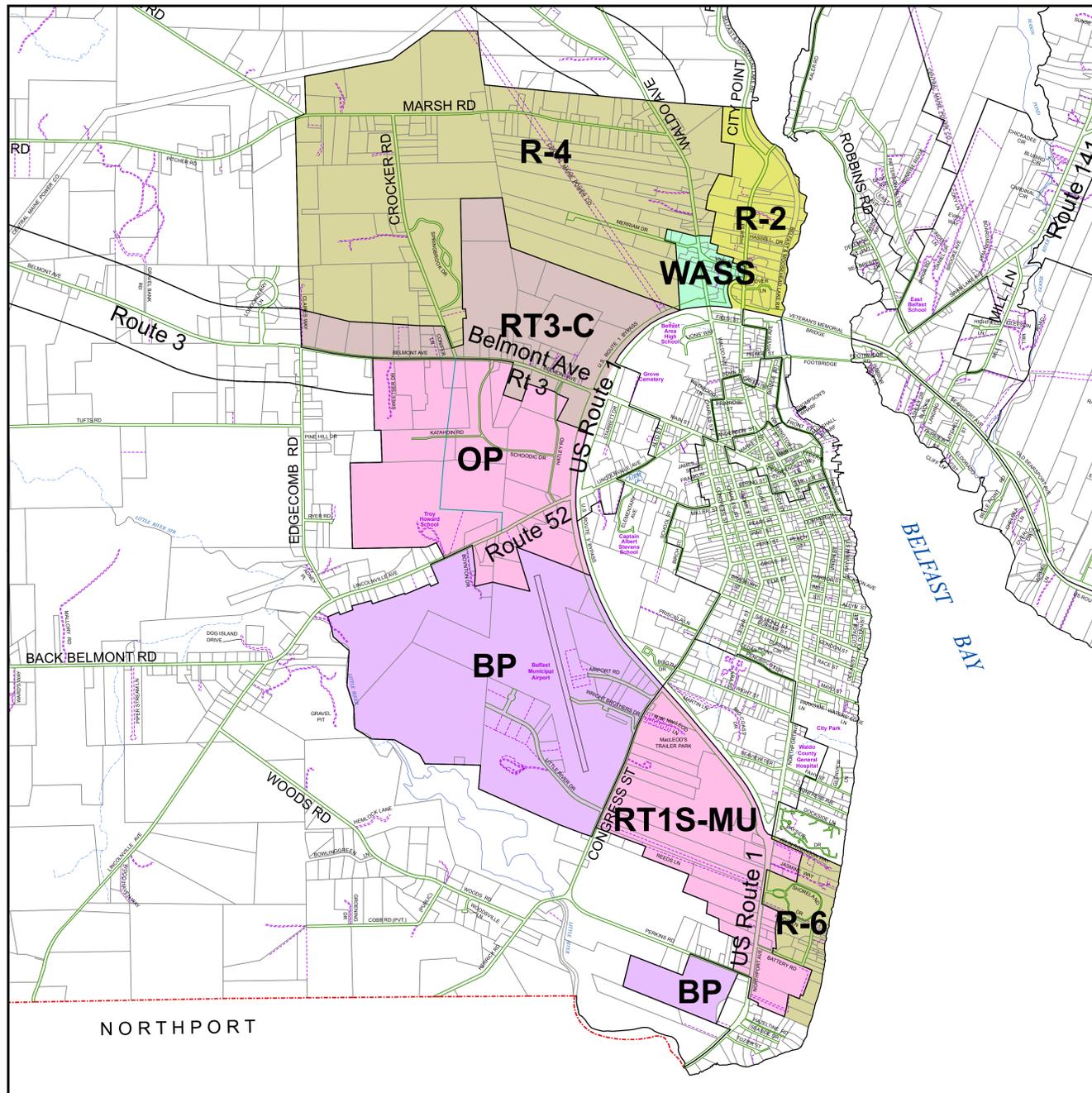


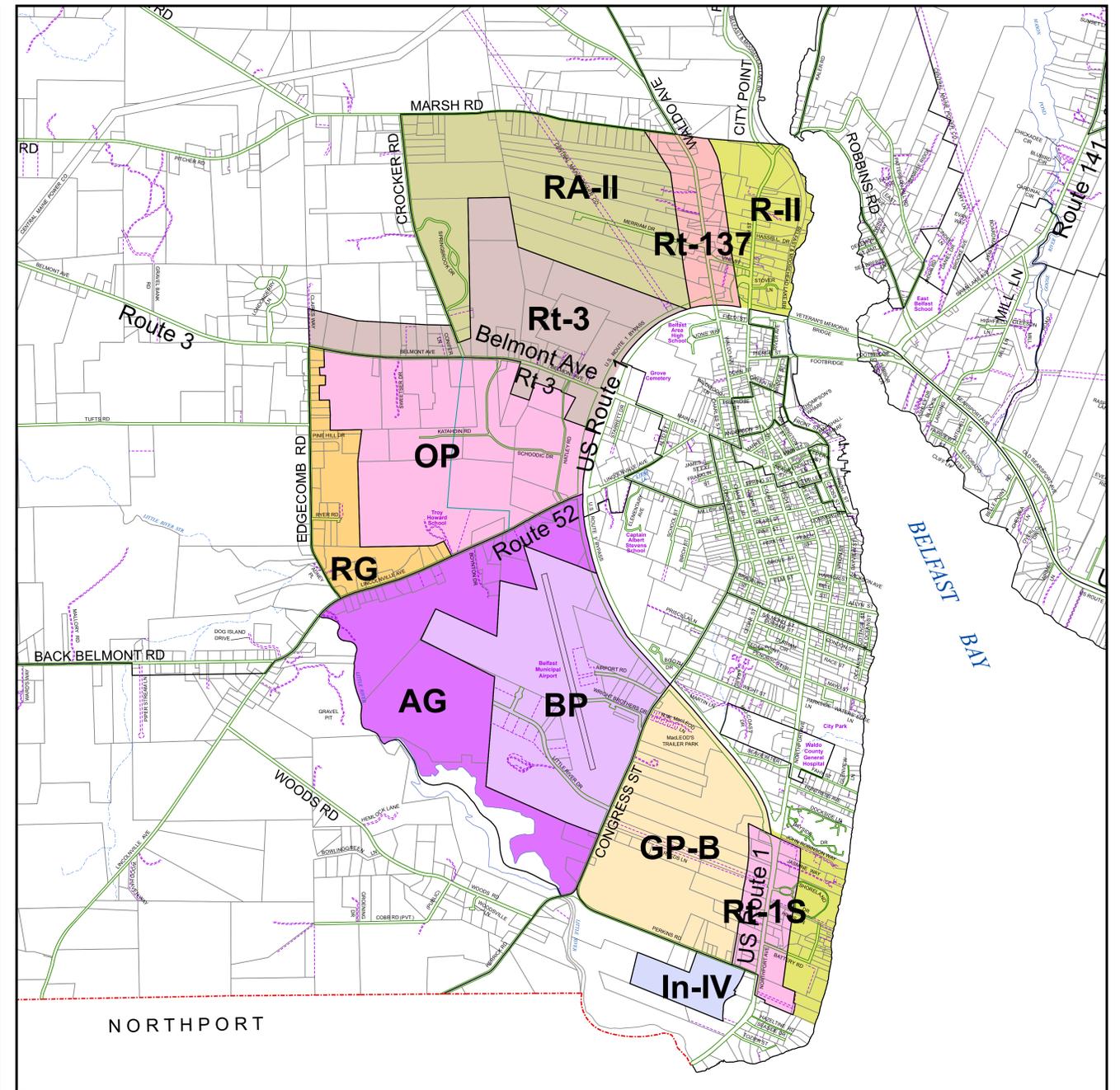
NEAR BYPASS

PROPOSED LAND USE AREAS



- Zone
- BP Business Park
 - OP Office Park
 - R-2 Residential 2
 - R-4 Residential 4
 - R-6 Residential 6
 - RT1S-MU Route 1 South – Mixed Use
 - RT3-C Route 3 Commercial
 - WASS Waldo Avenue – Small Scale

EXISTING ZONING



- Zone
- AG Airport Growth District
 - BP Business Park-Airport District
 - GP-B General Purpose "B" District
 - In-IV Industrial IV Perkins Road District
 - OP Office Park District
 - RA-II Residential/Agricultural II District
 - RG Residential Growth District
 - R-II Residential II District
 - Rt-137 Route 137 Commercial District
 - Rt-1S Route One South Commercial District
 - Rt-3 Route 3 Commercial District

RESIDENTIAL 4 AREA

“ENCOURAGE HOUSING”

Goal

To establish an area that is close to public services, shopping, jobs and existing housing which could accommodate much of the community’s long-term need for higher density residential housing.

Overview of Past, Current & Future Use

This area supports a mix of uses, but the predominant use is residential housing. Single family houses of varying values and sizes are the most common development. There also are several multi-family housing projects in the area; Belfast Square Apartments (senior housing) on Merriam Drive, the 46 unit Springbrook Condominium project on Crocker Road which MBNA initially constructed in 2000 to provide housing for its employees, and the Group Home Foundation’s 6 unit independent living apartments for persons with special needs on Waldo Avenue that was constructed in 2008. With the exception of the above noted developments, most houses are located on the main streets in the area, and little development is located more than 200 feet from the street. Most nonresidential uses in the area, such as B&J Auto Repair and Flagg’s Garage, are small in scale and create little traffic impact.

The City believes that this area presents an opportunity to satisfy many of the community’s long-term needs for higher density residential housing, and proposes this area to be a long-term residential ‘Growth Area’. This area has many of the features you look for in establishing a growth area; it is close to the downtown, schools, major places of employment, and current urban areas, and there is a significant amount of underdeveloped land. A significant drawback, however, is that most of the area is on the periphery of City sewer. That said, much of the area is on public water. As such, the City should be prepared to constructively work with persons who would like to develop this area for significant amounts of housing by financially participating in the extension of public services, particularly public sewer. Steep slopes, pockets of wetlands, and the narrow configuration of some of the larger parcels will present constraints, but the positive characteristics of this area outweigh the drawbacks.

Recommendations

Permitted Uses (Examples of Main Uses Permitted)

- 1) Single-family & two-family residences
- 2) Multi-family housing (sewer & by using a subsurface system)
- 3) Congregate Care & Elderly housing (sewer & by using a subsurface system)

- 4) Home Occupations, Larger Scale
- 5) Schools, Churches, Child Care & Similar Uses,
- 6) Small Scale Professional Offices
- 7) Indoor Recreational Facilities
- 8) Agricultural

Minimum Lot Size

- 1) 1/3rd acre – 15,000 sq ft for single-family or two-family (sewer)
- 2) 1/2 acre – 20,000 sq ft for single family (septic)
- 3) 1 acre – 40,000 sq ft for two-family (septic)
- 4) 1 acre for Multi-family (sewer or subsurface system)
- 5) 1 acre for any type of nonresidential use
- 6) 150 feet street frontage

Density (Number of Dwelling Units per Acre)

- 1) 3 single family units per acre (sewer)
- 2) 2 single family units per acre (septic)
- 3) 6 two-family units per acre (sewer)
- 4) 2 two-family units (1 structure) per acre (septic)
- 5) 12 – 16 multifamily units per acre if on sewer
- 6) Multifamily units on a subsurface system; density controlled by plumbing code
- 7) No specific density standard for elderly or congregate housing if on sewer, density controlled by plumbing code if on septic/subsurface system
- 8) No specific restriction on amount of lot coverage for either a residential or a nonresidential use

Setbacks (Distance Structure Must be Located from a Lot Line)

- 1) Single family and two family residential; 30 feet front and 15 feet side and rear
- 2) Multi-family residential; 30 feet front and 25 or more feet side and rear. Parking to be located to the side or rear of the structure.
- 3) Nonresidential use; 30 feet front and 25 or more feet side and rear. Parking to be located to the side or rear of the structure.

Major Changes Compared to Current Requirements

- 1) The proposed Residential-4 area includes land that is now in 3 different zoning districts; Route 137 Commercial, Residential Agricultural I and Residential Agricultural II. The intent is to create a district that allows similar uses on both sides of the major roads (Waldo Avenue, Crocker Road and Marsh Road), and to establish housing as the main permitted and desired use.
- 2) This proposed area would allow greater housing density than current zoning, particularly the number of multi-family units. This also means that the minimum lot size for a residential use is often less than current standards. In particular, the

Residential Agricultural I zone now requires a 2 acre minimum lot size for a house, but this proposed district will require a ½ acre lot size for a property that is not on public sewer, and 1/3rd acre for a property on sewer. It is noted that the proposed density standards (except for multi-family housing which is now only 6 units per acre) are now the norm in the Residential Agricultural II and Route 137 zones.

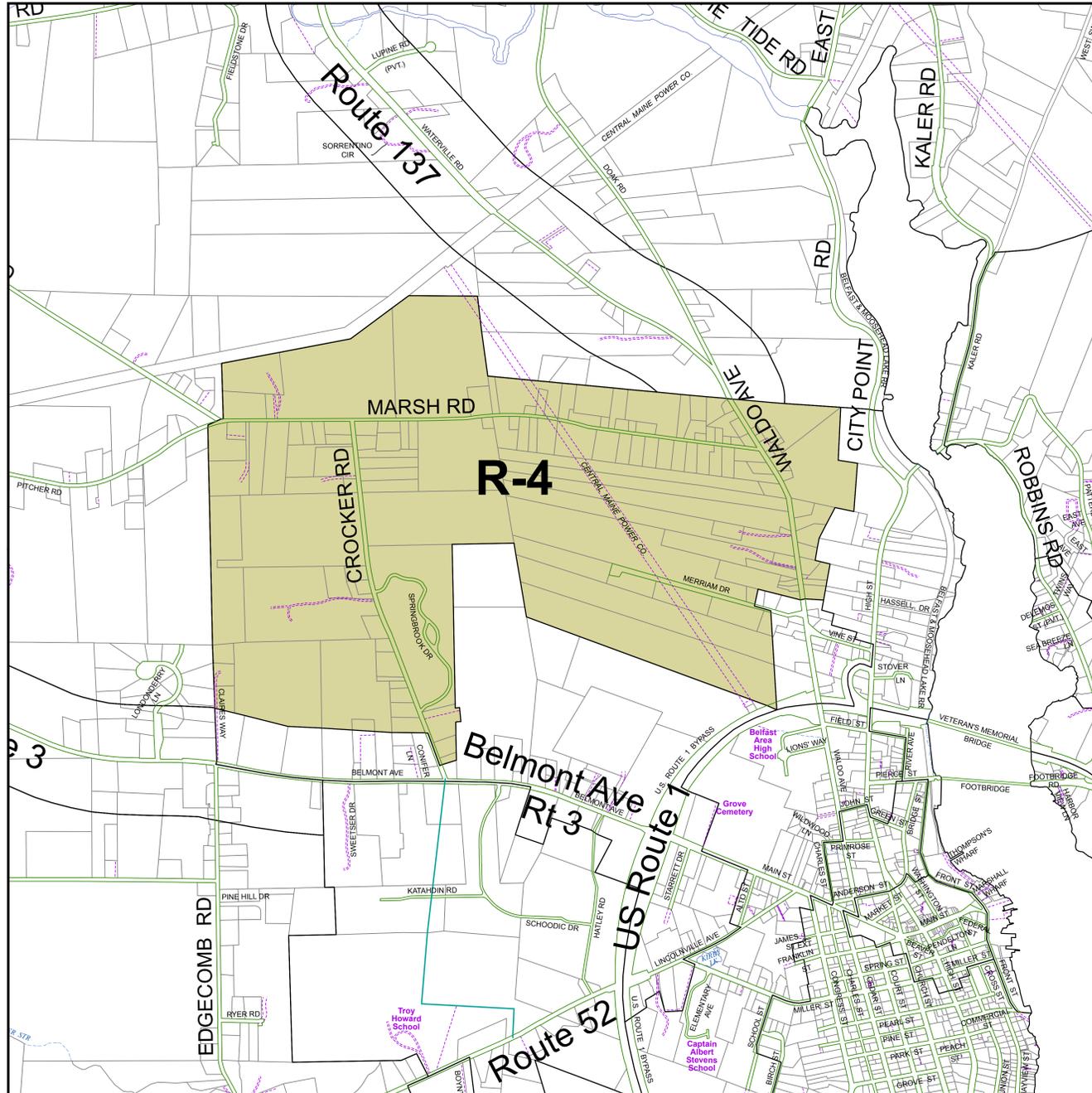
- 3) The proposed area eliminates many of the nonresidential uses that are now permitted, particularly in the area along Waldo Avenue that is now in the Route 137 Commercial zone.

Other Issues & Ideas To Consider

- 1) A major impediment to encouraging greater density housing development in this area is the lack of public sewer. The City should consider cooperating with a potential developer by financially participating in the cost of a sewer extension. City also could consider extending public sewer entirely at City cost along the section of Waldo Avenue out to Marsh Road that is not now served by sewer as a way to encourage growth.
- 2) City should consider allowing additional density and waiving sewer connection fees for a project that constructs affordable housing as a portion of the development.
- 3) Some of the area is quite steep. Elevation/grades in the area could inhibit the development of interconnecting roads among/between future developments.
- 4) City should ensure that several existing nonresidential uses that could be rendered nonconforming by the implementation of this recommendation have the ability to expand, including an increase in the size of the lot. The City wants these existing businesses to be able to thrive and expand.
- 5) The amount and speed of traffic in this area has been cited by residents as a concern. Route 137 is a State road that supports considerable commuter traffic, and Crocker Road appears to be experiencing more ‘cut-through’ traffic. The goal is to encourage most new housing to be located on new streets that will serve individual neighborhoods, such as the road constructed for Springbrook Condominiums, rather than relying upon single driveways located off the major streets for individual residences.
- 6) The area along Route 137 as far to the northeast as Marsh Road is in the MDOT urban compact zone. The inclusion of this area within the urban compact zone is one of the reasons which support identifying this as an area which can support greater housing densities.

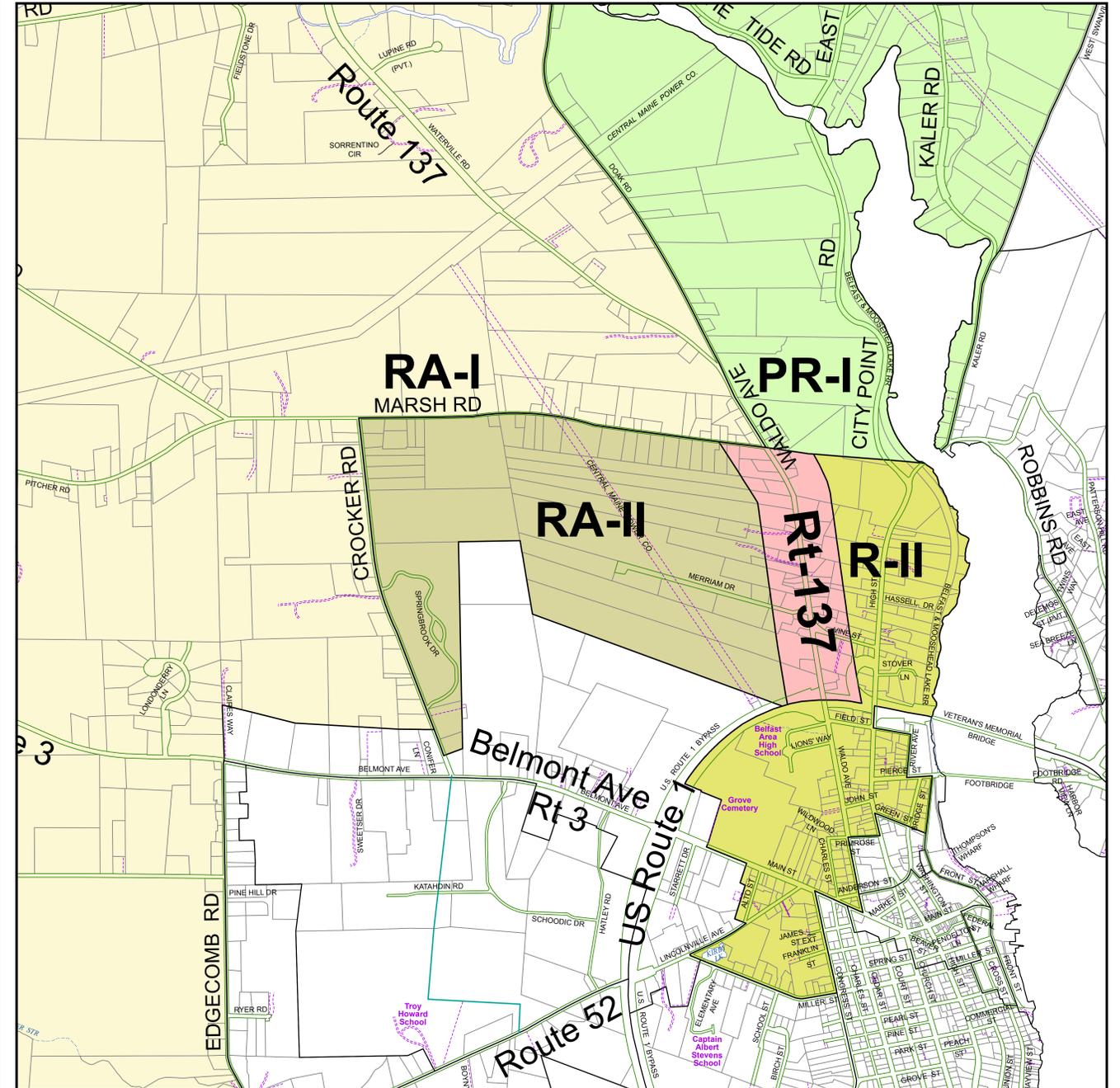
Residential-4

PROPOSED LAND USE AREA



Zone
R-4 Residential 4

EXISTING ZONING



Zone
PR-I Protection Rural District
RA-I Residential/Agricultural I District
RA-II Residential/Agricultural II District
R-II Residential II District
Rt-137 Route 137 Commercial District

WALDO AVENUE SMALL SCALE MIXED USE AREA

“SMALL BUSINESS IS KEY”

Goal

To establish an area located near the downtown/in-town area in which small scale business development is encouraged.

Overview of Past, Current & Future Use

The area near the Route 1 overpass now supports a number of small scale businesses, and in the 70's was one of the areas that supported the City's chicken processing industry. Currently, Perry's Quick Stop, Waldo Avenue Redemption Center, KSW Credit Union, the Wentworth professional office complex, Krabriel/Riposta Funeral Home, and Lisa Nichols Hair Salon are located along Waldo Avenue/Route 137 between Merriam Drive and the Route One overpass. On High/Vine Street, there is Belfast Variety, and a monument business is located on Vine Street. Single family houses and an occasional duplex are located on most other lots.

This small area experiences a considerable amount of traffic. This is mostly because the on/off ramps for Route 1 are located in the area; with the southerly on-ramp to Route 1 directing cars to Route 3, and this being the main exchange for persons traveling on Route 137 and Route 7. The amount of traffic helps support the type of businesses that are located in the area, and the proximity of several busy intersections detracts from the suitability of this area for residential growth. City regulations should allow a wide variety of small scale businesses, while working to ensure such businesses positively contribute to the character of the community and the area.

This clearly is a mixed use 'Growth Area' that could support additional small business development and limited amounts of additional housing. The availability of public sewer and public water on Vine Street and to most properties near Vine Street and Merriam Drive on Waldo Avenue also supports this area being a 'Growth Area'. It is also noted that this section of Route 137 is located within the MDOT Urban Compact area. Lastly, the uses permitted in this area, and the performance standards that should be used to regulate development should be very similar to those used for the proposed Searsport Avenue Small Scale district located near the Route 1/Route 141 intersection.

Recommendations

Permitted Uses (Examples of Main Permitted Uses)

- 1) Small Scale Retail, perhaps up to 20,000 square feet in size, however a lesser sized retail use may be more appropriate.
- 2) Service Businesses & Professional Offices
- 3) Restaurants, however fast food restaurants would be prohibited. The exception to this standard would be a fast food restaurant (no drive-through) that is established as an accessory use to another business, such as a restaurant franchise that is located or operated in a convenience store/service station.
- 4) Auto Repair and Service, with auto sales as an accessory use.
- 5) Service Stations
- 6) Single-family & two-family residences
- 7) Multi-family housing
- 8) Home Occupations, Larger Scale
- 9) Schools, Churches, Child Care & Similar Uses

Minimum Lot Size

- 1) 1/3rd acre – 15,000 sq ft for single family or two-family (sewer)
- 2) 1/2 acre – 20,000 sq ft for single family (septic)
- 3) 1 acre – 40,000 sq ft for two-family (septic)
- 4) 1 acre for Multi-family, (housing units must be on sewer)
- 5) 1 acre for any type of nonresidential use
- 6) 150 feet street frontage, minimum, for residential and nonresidential uses

Density (Number of Housing Units per Acre)

- 1) 3 single family units per acre (sewer)
- 2) 2 single family units per acre (septic)
- 3) 6 two-family units per acre (sewer)
- 4) 2 two-family units per acre (septic)
- 5) 12 - 16 multifamily units per acre (provided served by public sewer)
- 6) No specific restriction on amount of lot coverage for either a residential or a nonresidential use

Setbacks (Distance Structure must be Located from a Lot Line)

- 1) Single family and Two-family Residential Uses: 30 feet front, 15 feet side & rear.
- 2) Multi-family Residential: 30 feet front and 25 feet side & rear. Parking to be located to side or rear of building.
- 3) Nonresidential uses: 30 feet front for nonresidential uses and 15 feet side & rear for nonresidential uses, unless the use abuts an existing residential use which would increase the side and rear structure setback requirement to 25 feet.

Major Changes Compared to Current Requirements

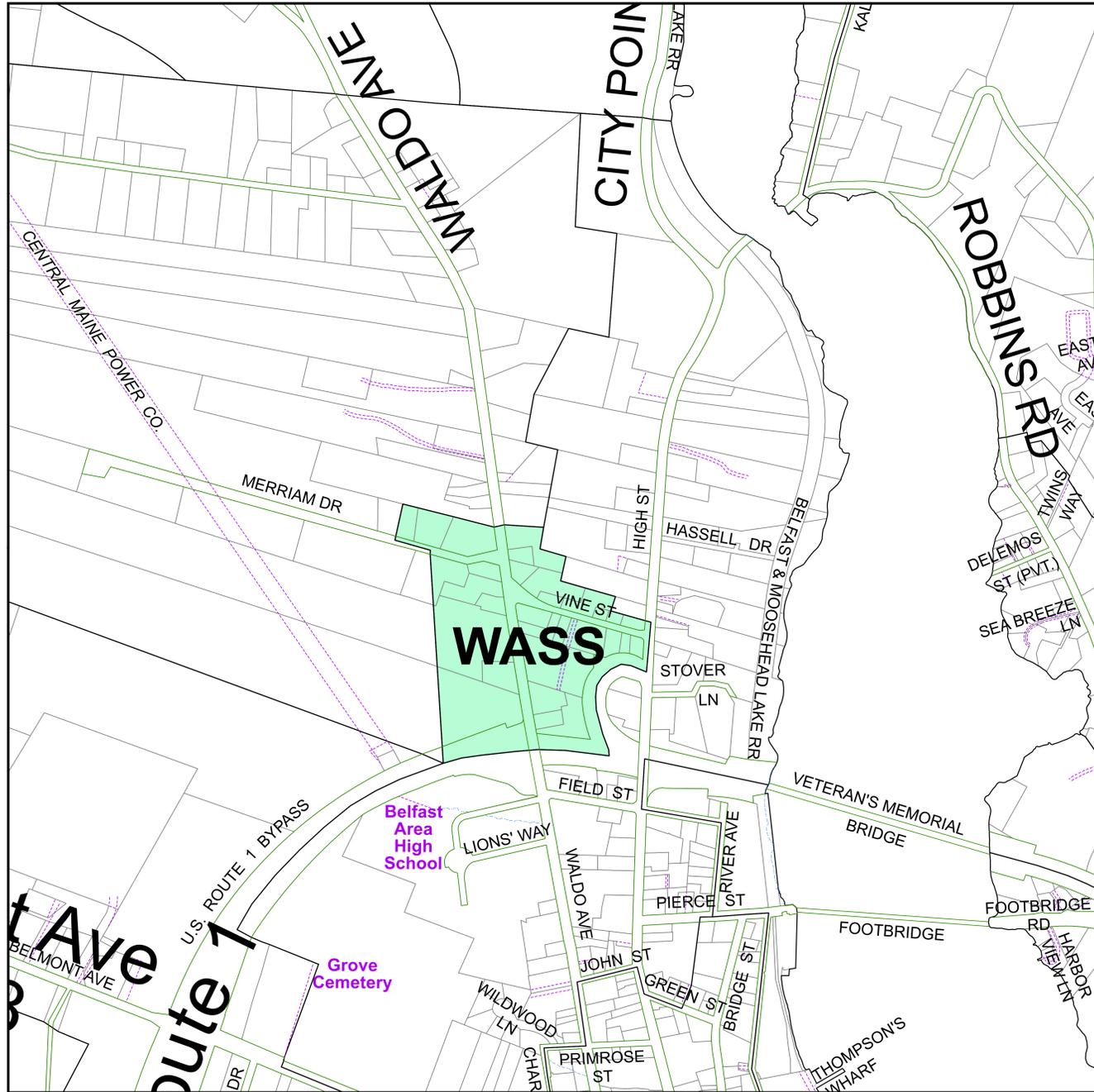
- 1) It is also noted that the amount of area along Waldo Avenue that is included in this proposed area has been greatly reduced. At present, the area along Waldo Avenue from the Route 1 overpass to Marsh Road is in the Rt 137 Commercial zone; a zone which is 500 foot deep on both sides of the road. The new westerly boundary for this mixed use area along Waldo Avenue is proposed to be near Merriam Drive, which eliminates about 2,000 feet of road frontage along Waldo Avenue from this area.
- 2) A second proposed boundary change is to include all of Vine Street, including Belfast Variety, within this mixed use small scale area. At present, this area is in the Residential II zone, which prohibits all retail operations. Belfast Variety is now considered a non-conforming use, but it would become a conforming (allowed) use if this proposal is adopted.
- 3) In comparing current zoning to this proposal, little change is recommended in the types of nonresidential uses that would be allowed.

Other Issues & Ideas To Consider

- 1) Consider amending the current nonresidential performance standards that apply to this area to eliminate standards that require an increase in lot frontage or greater setbacks based on the size of a nonresidential structure or the amount of traffic generated.
- 2) While City sewer is available to most properties, sewer is not now available to several of the properties located easterly of Vine Street, such as the Redemption Center. City should consider extending public sewer to this area and should be willing to participate in the cost of extending this service.
- 3) The area along Route 137 (Waldo Avenue) is now inside the urban compact zone which means that this section of Route 137 is not subject to MDOT access management requirements. The City, not MDOT, regulates most access management concerns.
- 4) The City should ensure performance standards that apply to nonresidential uses in this area help minimize conflicts between existing residential and non-residential uses. This is important in a mixed use area.
- 5) The width of this area is deliberately quite narrow; a maximum width of 500 feet along Waldo Avenue. The City believes that additional depth is not needed to support the type of nonresidential development targeted for this area.

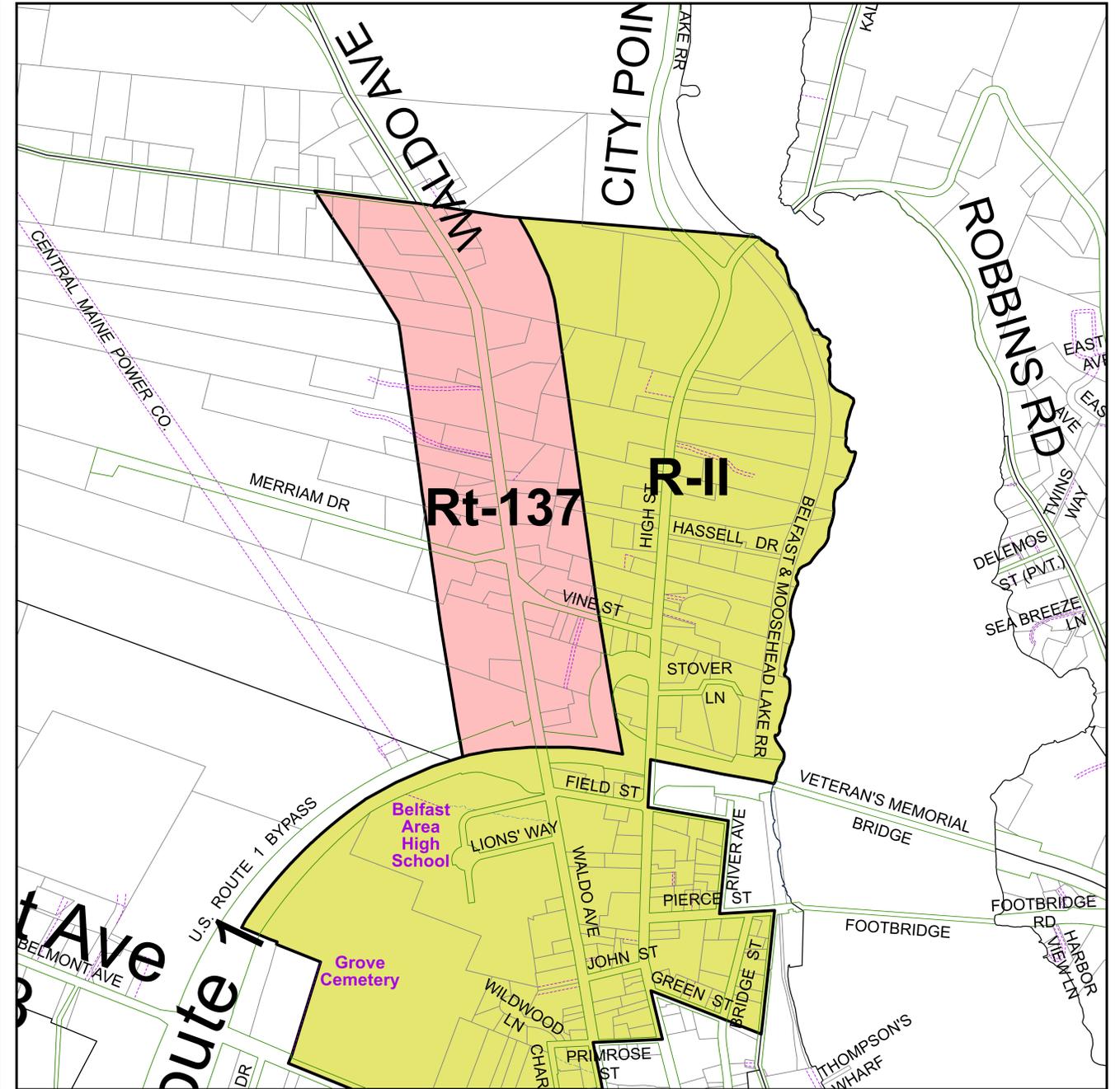
Waldo Avenue – Small Scale

PROPOSED LAND USE AREA



Zone
WA-MU Waldo Avenue – Mixed Use

EXISTING ZONING



Zone
R-II Residential II District
Rt-137 Route 137 Commercial District

ROUTE 3 COMMERCIAL ZONE

“MAIN SHOPPING AREA”

Goal

To provide an area in which retail, including larger retail and fast food restaurants, can prosper and serve as a prime shopping area for Belfast and County residents. Further, to establish strict boundaries for this area to ensure compact development and to lessen the likelihood of commercial sprawl.

Overview of Past, Current & Future Use

This area, which is located along Route 3, Lincolnvile Avenue and Starrett Drive near the intersection of Routes 1 and 3, is a main shopping area in Belfast and now supports most of the community’s larger retail stores, fast food restaurants, and similar stores owned by national/state/regional interests. The City should adopt policies which allow and attract such uses to this area. The City believes the area is well-suited for this purpose because of its location near major State roads and the accompanying amount of traffic. Also, this is where businesses have chosen to locate, there is additional land to support commercial development, and commercial development in this area is less likely to adversely affect residential uses. The City also should use performance standards and flexible zoning approaches to manage impacts associated with development and ensure that new development incorporates good quality design features that complement the City’s character.

The community debated the future use of this area at length in 2000 – 2001 and again in 2007 – 2008. In October 2007, and February 2008, the City Council adopted amendments to the zoning regulations that were adopted in 2001. The 2001 regulations recognized this area as a major commercial district, but limited the maximum size of retail to 75,000 square feet. The amended 2007/2008 regulations clarified the type of uses permitted in the area, the performance standards and other regulatory tools that would be used to manage development, and the boundaries of this area. The key components of the new standards are as follows:

- City will allow 1 retail store greater than 75,000 square feet that sells clothing, general merchandise and/or groceries in the area located on the northerly side of Route 3, between Route 1 to the east and Crocker Road to the west.
- Development of the so-called ‘Les Hills’ property, that is now owned by Bob Bahre, must involve the preparation of a master plan and is subject to contract rezoning.
- Crocker Road shall serve as the westerly boundary for retail and restaurant development, with an exception granted for the Perry’s Furniture property.

- The City increased the depth of this zone on the northerly side of Route 3 between Crocker Road and Route One.
- Most other zoning standards adopted in 2001 continue to apply to the remainder of the area.

It is also noted that few residences are now located in this area; there are only 5 residences located easterly of Crocker Road. This is much different than most other commercial areas in Belfast, as most commercial areas have as many if not more residences than businesses. The lack of residences in the area is one of the key reasons why this area was chosen to allow larger businesses.

Stated simply, this district is the primary commercial 'Growth Area' for Belfast.

Recommendations

These recommendations reflect the new zoning standards the City Council adopted in October 2007 and March 2008.

Permitted Uses (Examples of Main Uses)

- 1) 1 Retail Store larger than 75,000 sq ft that sells clothing, general merchandise and/or groceries in area between Route 1 and Crocker Road.
- 2) Retail, up to 75,000 sq ft per structure, in other areas located easterly of Crocker Road. No new retail located westerly of Crocker Road
- 3) Restaurants, including fast food, except no restaurants are permitted westerly of Crocker Road
- 4) Service businesses
- 5) Hotel
- 6) Manufacturing
- 7) Professional Offices
- 8) Single family residential and two-family residential

Minimum Lot Size

- 1) 1 acre (sewer or septic) for a residential or nonresidential use
- 2) Minimum 150 ft of street frontage for a residential or nonresidential use, with an increase in amount of frontage required based on size of nonresidential development or number of vehicle trips generated per peak hour.

Density (Number of Housing Units per Acre)

- 1) 1 single family unit or 1 two family unit per acre
- 2) No specific restriction on amount of lot coverage for either a residential or a nonresidential use.

Setbacks (Distance Structure must be Located from a Lot Line)

- 1) Residential uses – 30 feet front setback and 15 feet side and rear
- 2) Nonresidential uses have a minimum front setback requirement of 30 feet with increases based on the size of the structure.
- 3) Nonresidential uses have a minimum 15 feet side and rear setback with increase based on size of development
- 4) Contract rezoning applies to the ‘Les Hills’ property, which means that setback requirements are flexible.

Major Changes Compared to Current Requirements

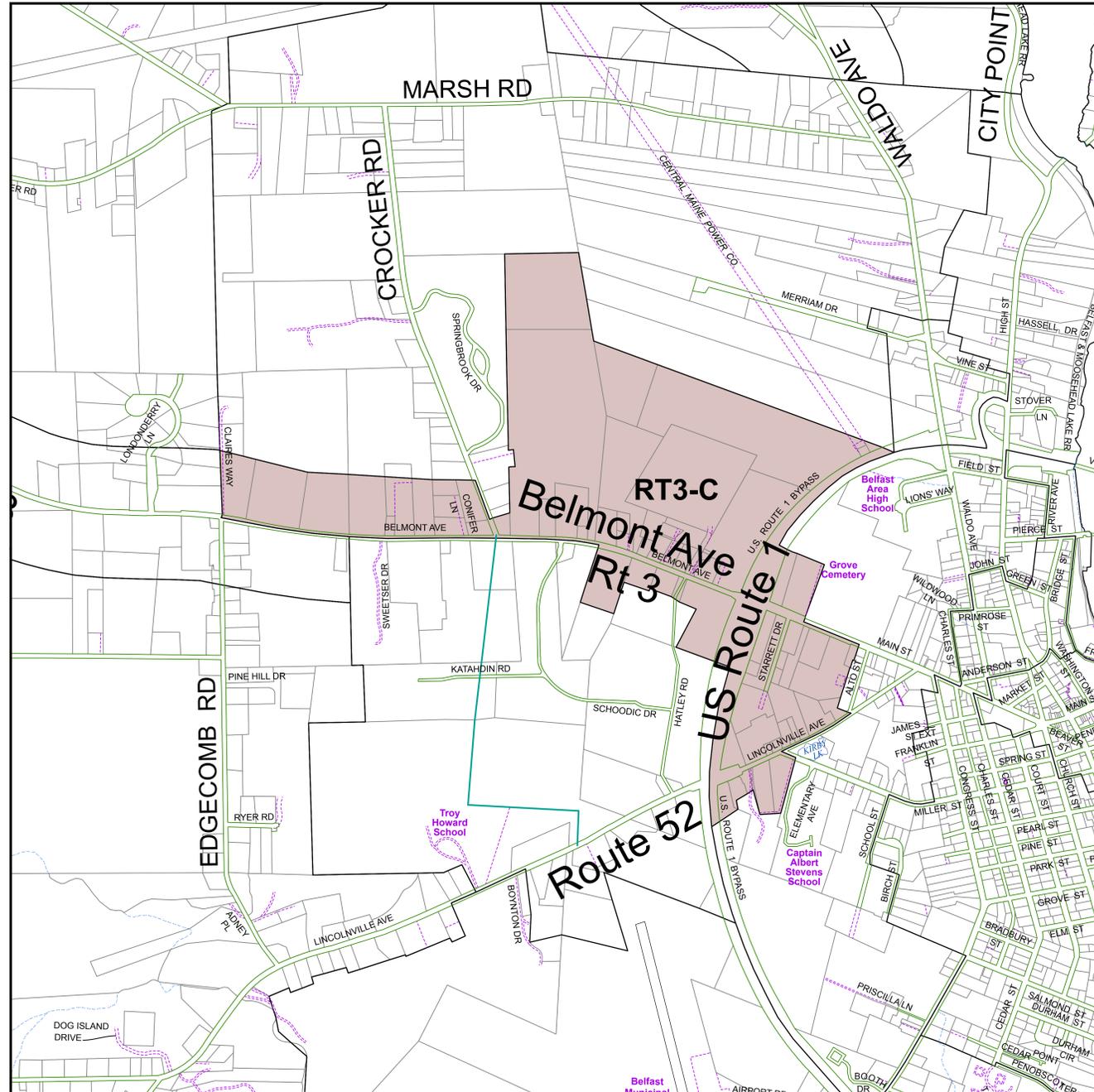
No new changes are now proposed. City notes that extensive changes were made to the Comprehensive Plan and also the City Zoning Ordinance when the Council adopted amendments for this area in late 2007 and early 2008. These policy decisions and the zoning adopted to implement this policy should remain in effect.

Other Issues & Ideas To Consider

- 1) Potential need for new signal light and road improvements at the Schoodic Drive entrance to Bank of America property if retail development occurs on ‘Les Hills’ property.
- 2) No public sewer is now available beyond the Schoodic Drive entrance to Bank of America. With the City’s intent to limit significant commercial development to the area located westerly of Crocker Road, the City should discourage the extension of public sewer westerly of Crocker Road.
- 3) Quite a bit of the property along Route 3 consists of wetlands, which could inhibit the scale of future development.
- 4) In the long-term, if the ‘Les Hills’ property is fully developed, the City may want to consider expanding the depth of the Route 3 Commercial zoning district toward Route 137 (Webber property) to support additional commercial development, provided the main means of access to this area is from Route 3, and the prohibition on larger retail along Waldo Avenue remains.

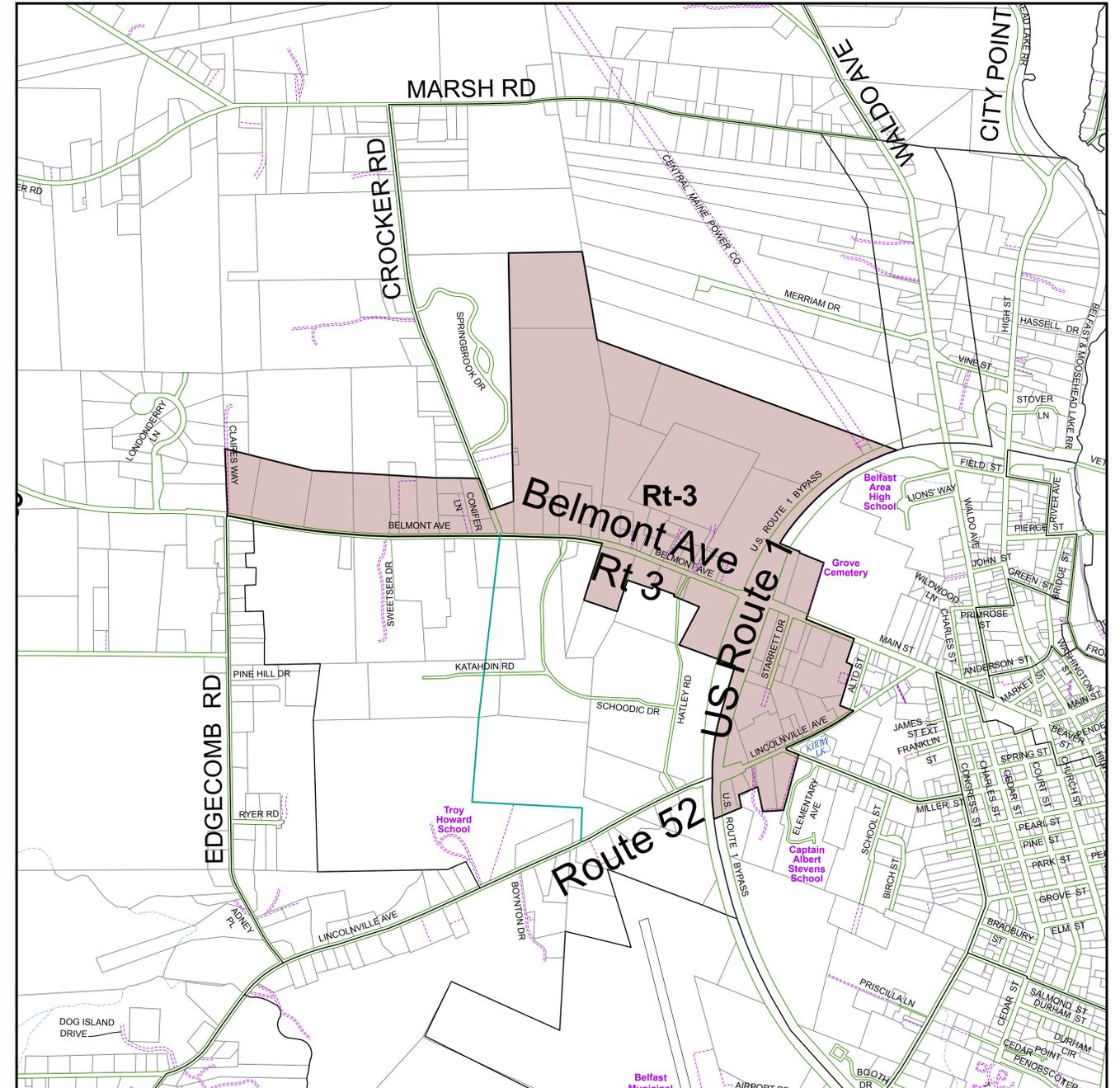
Route 3 Commercial

PROPOSED LAND USE AREA



Zone
RT3-C Route 3 Commercial

EXISTING ZONING



Zone
Rt-3 Route 3 Commercial District

OFFICE PARK

“JOBS ZONE”

Goal

To create an area in which large scale office and job based development is encouraged, and to ensure that other development permitted in the area complements this type of development.

Past, Current & Future Use

The character and use of this area changed dramatically in the mid-1990's when MBNA decided to purchase the former farm fields and residences and to develop a large call center operation. Within the short span of 7 years, what was once open space was developed into nearly 600,000 square feet of professional office space and support facilities, including the University of Maine Hutchinson Center. At its peak, MBNA employed about 2,800 persons. MBNA ceased operations in 2006 when it merged with Bank of America. Bank of America continues to employ about 1,500 persons, and in 2008, sold Phase I of the office complex to athenahealth, inc., which is using the facility to support its medical/insurance payment processing operations. As of August 2009, athenahealth employed 220 persons and was planning to add more employees.

The City, in 1985, identified this area as a primary site for large scale job creation activities when it located most of the area in an Industrial zone. In 2001, the City amended the types of uses permitted in the area and the boundaries of the zone to reflect MBNA's development. More recently, in 2007 – 2008, during the Council's debate of the 'larger retail' issue, the City revised the boundaries of the zone that were established in 2001 and made several changes to the types of uses permitted. It is proposed that most of the 2007 – 2008 changes remain in effect.

This area also supports a mix of larger scale public and non-profit uses, including Troy Howard Middle School, the YMCA, Sweetser Homes, Inc., the aforementioned University of Maine Hutchinson Center, the City ballfields, and the dog park that was constructed in 2008. There are a very limited number of privately owned homes in the area.

Further, this area is on the edge of Belfast Airport. It is important to regulate uses that occur in this area to minimize potential conflict with current and future airport operations. As such, the location and amount of residential development should be controlled.

This clearly is a 'Growth Area'. This small area supports nearly 1/3rd of the jobs in Belfast, and there are limited opportunities for additional complementary development to

support further job growth. History has shown that the City acted wisely nearly 25 years ago in reserving this area for development that created significant job growth.

Recommendations

Permitted Uses (Examples of Main Uses)

- 1) Offices and Office Complexes
- 2) Light industry/manufacturing
- 3) Retail, provided it is located easterly of a line parallel to Crocker Road. Further, City should allow retail development in this area consistent with the zoning guideline adopted in 2001; allow a maximum of 75,000 square feet of retail per 25 acres of property (reference Section 102-775(a) of current zoning code for the Office Park zoning district
- 4) Schools, child care and churches
- 5) Indoor and Outdoor Recreational facilities
- 6) Single family & two-family housing, provided the location is controlled to reflect airport operations (airport overlay district)
- 7) Home Occupations, larger scale.

Minimum Lot Size

- 1) ½+ acre for residential (greater for duplex if no sewer)
- 2) 1 acre (sewer or septic) for any type of nonresidential use.
- 3) Minimum 150 ft of street frontage with increase in amount of frontage required for nonresidential use based on size of development or number of car trips generated per peak hour.

Density (Number of Housing Units per Acre)

- 1) 1 single family residence per ½ acre (sewer or septic)
- 2) 2 two-family units (1 duplex) per ½ acre (sewer)
- 3) 2 two-family units (1 duplex) per acre (subsurface system)
- 4) Special exception should be granted for Sweetser Homes property on Route 3 and for potential dormitory construction associated with University of Maine Hutchinson Center
- 5) No specific restriction on amount of lot coverage for a residential or a nonresidential use.

Setbacks (Distance Structure must be Located from a Lot Line)

- 1) Residential; minimum 30 ft front setback and 15 foot side and rear setback.
- 2) Nonresidential; minimum front setback of 30 feet with increase based on size of development/structure.
- 3) Nonresidential; minimum 15 feet side and rear setback with increase based on size of development/structure.

Major Changes Compared to Current Requirements

- 1) The amendments that the Council adopted to the Office Park zoning district in 2007 – 2008 resulted in a significant change in the boundaries of this zoning district. The district formerly included properties owned by MBNA that were on the northerly side of Route 3. These areas were removed from the Office Park zone through adoption of the 2007 – 2008 amendments. It is noted that these areas had less likelihood of being developed for office use because they had been sold by Bank of America.
- 2) The proposal would result in eliminating multi-family housing as an option, but would still allow single family and two family housing, provided the location of the housing does not conflict with airport operations. This proposal, if implemented, would more clearly create opportunities for the Hutchinson Center to establish dormitory style housing.
- 3) The amount of land included within this area would be expanded to include several properties on the southerly side of Lincolnville Avenue/Route 52. These properties are now in the Airport Growth zone; a zoning district which is proposed to be eliminated. This change in classification could result in allowing the construction of a house in an area in which all new housing is now prohibited.
- 4) Eliminate the former 40+ acre ‘Gould/Perry’ property on Route 3 now owned by Bank of America from the Office Park zone and include it in the proposed Outside Rural, Rural Road Class 1 area, with the provision that this property could be used for any non-retail use permitted in the Office Park, or for higher density housing, through a Special Exception process administered by the Zoning Board of Appeals.

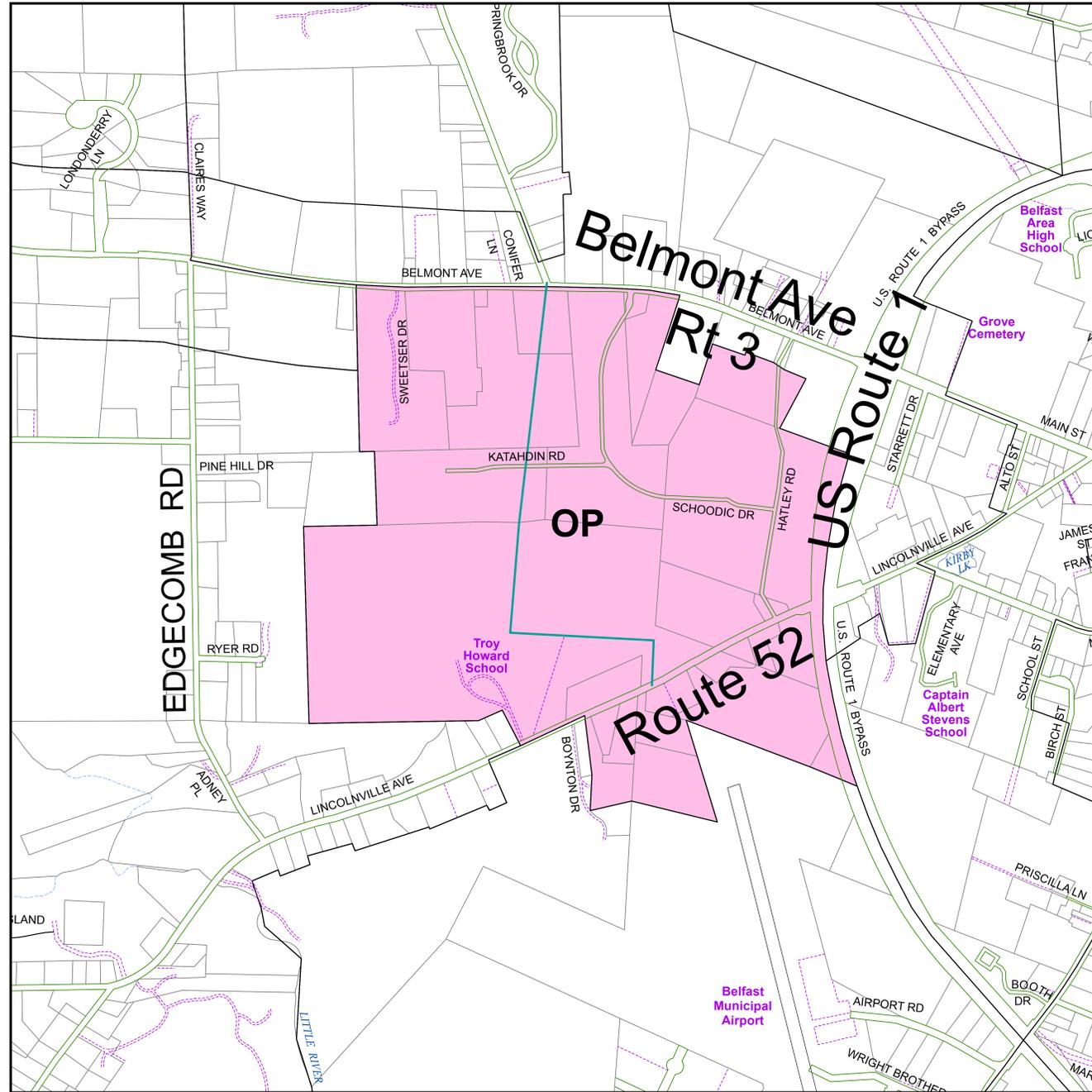
Other Issues & Ideas To Consider

- 1) It is important to manage conflicts with future use of airport by establishing an Airport Protection Overlay zone. The intent of this overlay zone would be to manage incompatible development with airport use. The proposed overlay district is a similar by different tool than the Airport Growth zone which the City established in 1997 that could be used to protect airport operations.
- 2) Most properties along Route 52 and some properties along Route 3 are subject to MDOT access management rules. Additional growth along Route 52 could result in more of this area being included in the City’s urban compact zone (MDOT designation), which means that the City may become responsible for the maintenance of this road.

- 3) A significant amount of the property in this area is comprised of wetlands. For example, Bank of America and athenahealth, inc own considerably more ground than the property on which their current buildings and parking lots are located, however, nearly all remaining land is wetland and cannot be used for significant amounts of additional development.
- 4) There is limited availability of public sewer in this area. The Troy Howard Middle School and YMCA on Route 52 are served by a force main (pump) line, and the properties owned by Bank of America, athenahealth, inc, and the University of Maine Hutchinson Center are served by privately owned lines. The lack of public sewer will affect the intensity of development that can occur on other properties. That said, this area likely is a low priority candidate for a public sewer extension via a gravity line.

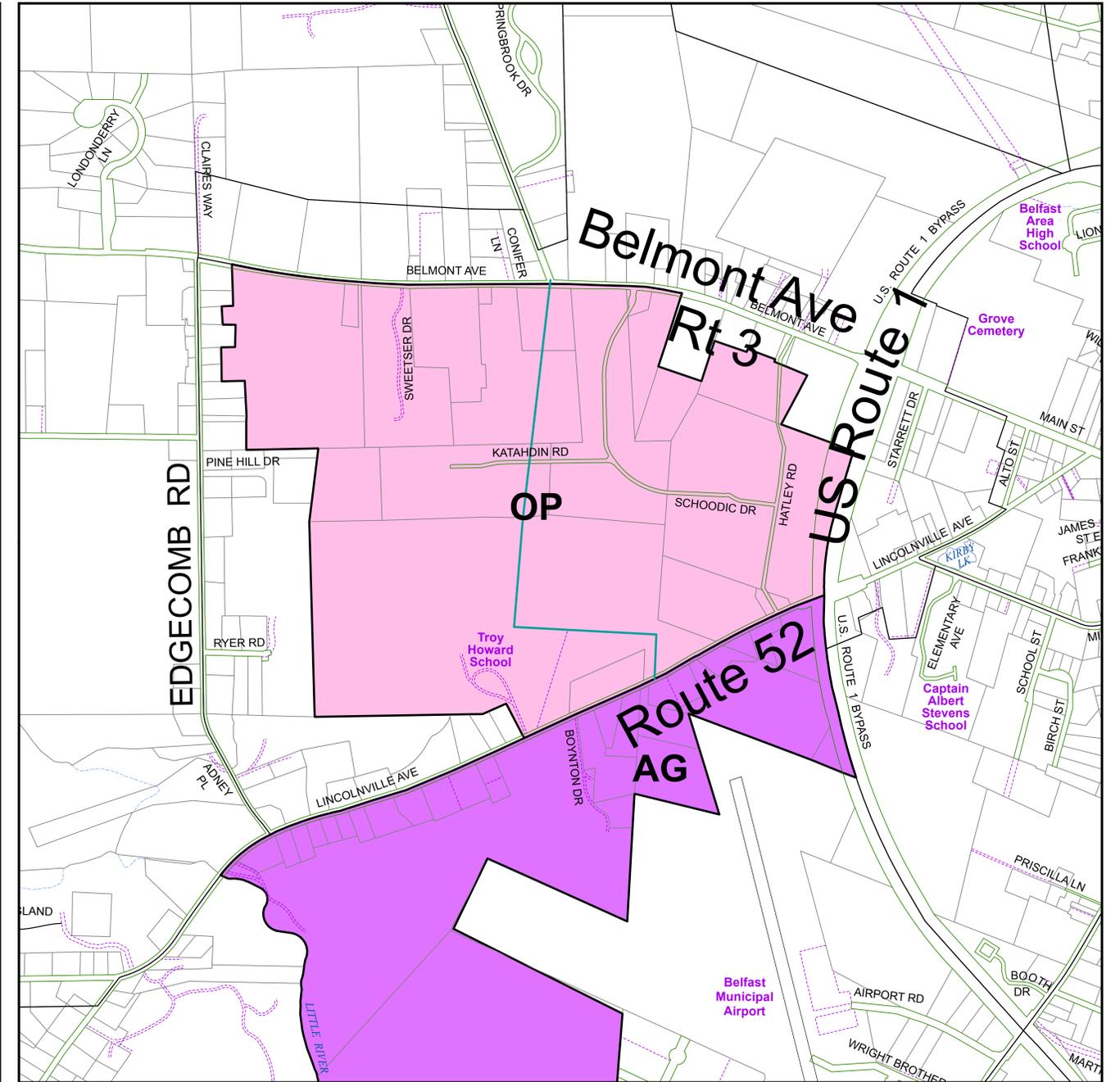
Office Park

PROPOSED LAND USE AREA



Zone
OP Office Park

EXISTING ZONING



Zone
AG Airport Growth District
OP Office Park District

BUSINESS PARK

“JOBS ZONE”

Goal

To establish an area that encourages business development within the City developed Business Park and on surrounding lands and which allows and supports airport operations and the expansion of such.

Overview of Past, Present & Future Use

The City established the Belfast Business Park, formerly the Belfast Industrial Park, in the mid-1980's. Current development in the Park, such as Ducktrap Seafoods and Belfast Industries, employs over 300 persons. The City owned 6 unsold lots in the Park in 2009, the combined size of which is about 12 acres. The Business Park surrounds the Belfast Airport, a municipally owned airport that is experiencing increasing private use and which underwent significant improvement in 2005 – 2006. At present, however, there are no commercial carriers which provide regular/scheduled passenger or freight services at the airport.

The proposed district includes all lands in the Business Park and the Municipal Airport, and also includes the 100 acre parcel the County purchased in 2004 from Charles Boynton as a potential site for a new County jail, and other lands owned by the Boynton's. The main purpose of this area is to encourage business growth that creates jobs, particularly industry, warehousing and offices. The City Council, in 2004, amended terms of the former Industrial I zone to establish the Business Park zone. Current regulatory standards appear to be appropriate for this area. This approach recognizes that the City covenants that regulate the use of properties in the Park often provide more definitive standards to manage uses in the Business Park and at the Airport than via the use of zoning regulations.

In addition, Matthews Brothers' current operations on Perkins Road are now located in a zoning district that applies only to their property; the Industrial IV zoning district. It is recommended that this property be included in the Business Park area. This change would not adversely affect Matthews Brothers operations and would result in the elimination of a single property zoning district.

Recommendations

Permitted Uses (Examples of Main Uses)

- 1) Manufacturing, including accessory retail
- 2) Warehousing
- 3) Offices & some service businesses
- 4) Municipal & County uses
- 5) Single family (limited to 1 unit per parcel or 1 unit per every 5 acres for parcels greater than 5 acres. Also, no residential uses in the City Business Park or at the Airport per existing covenants).

Minimum Lot Size

1 acre (sewer or septic) for any use
Minimum 150 ft of street frontage

Density (Number of Housing Units per Acre)

A maximum of 1 single family residence per lot, or if a parcel is greater than 5 acres in size, 1 housing unit per 5 acres. This density requirement reflects concern with the potential conflict between residential uses and use of Belfast Airport. It is also noted that residential uses are prohibited in the Business Park.

Setbacks (Distance Structure must be Located from a Lot Line)

- 1) Setback requirements for the Business Park are now established by Business Park regulations, and this approach should continue.
- 2) Setbacks for other areas should be no less than 25 feet from all lot lines, with the current front setback provision for the Matthews Brothers property (Industrial IV zone) remaining in effect.

Major Changes Compared to Current Requirements

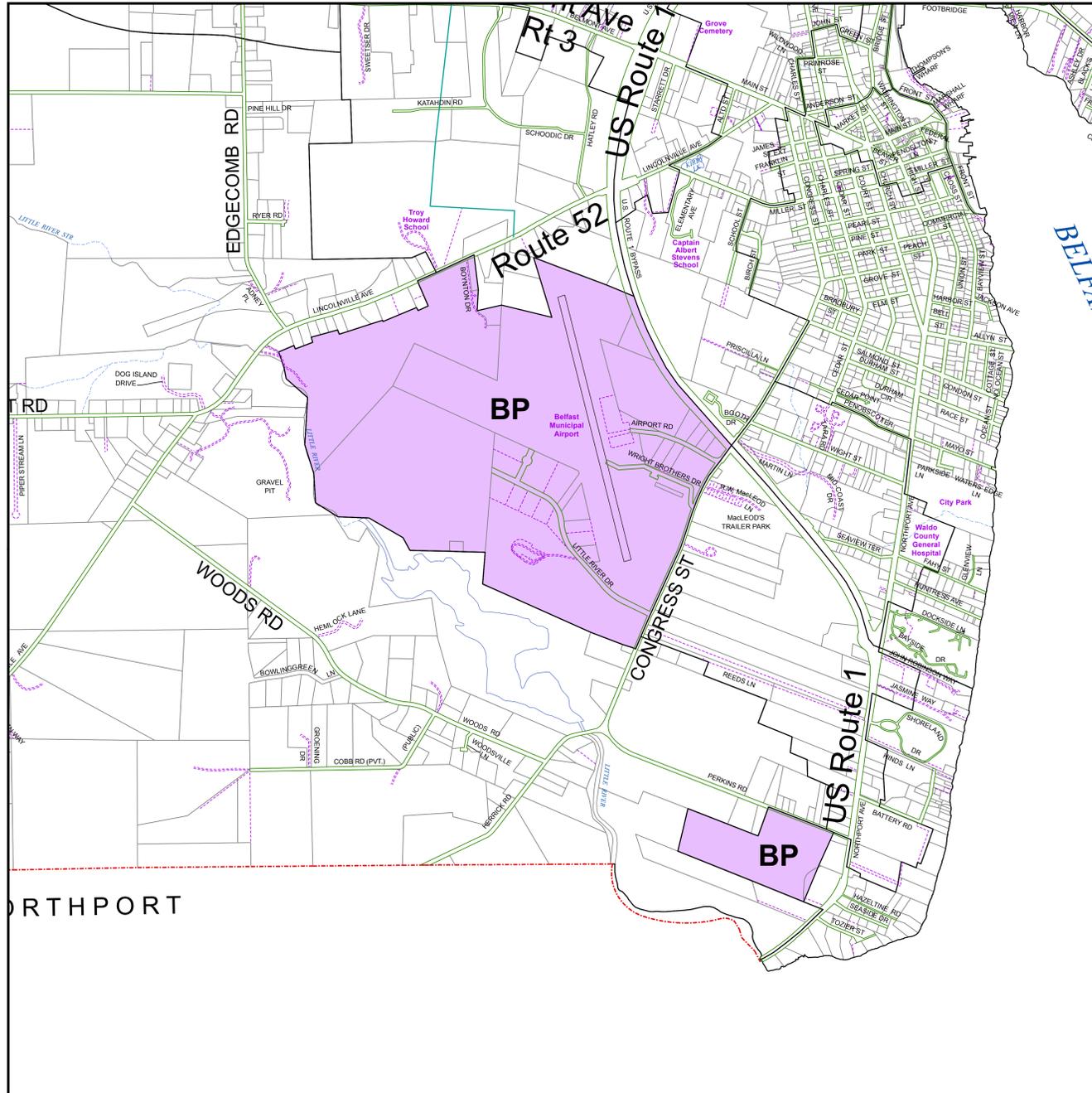
- 1) One change is to extend the boundaries of this area to properties now owned by the Boynton family and Waldo County that currently are in the Airport Growth zone.
- 2) Second change is to allow a maximum of 1 single family house for every 5 acres of land. Housing is now prohibited. It also is noted that housing is expressly prohibited at the Business Park and the Airport.
- 3) Proposal also should apply to the Mathews Brothers property on Perkins Road, and should replace the current Industrial IV zone, which is a single property district.

Other Issues & Ideas To Consider

- 1) It is important to better manage potential land use conflicts regarding current and future use of the airport by establishing an Airport Protection Overlay zone. The intent is to establish an overlay district that accurately reflects areas in which conflicts could occur, and regulations that effectively address these conflicts. The City Airport Committee should be consulted in establishing the boundaries and regulations that would apply to the overlay district. This proposed overlay district would replace the current Airport Growth zone area.
- 2) The City likely will need to upgrade the public sewer in the Business Park if a high volume water user locates at the Park. The current sewer can support low level users, but a high level user could require upgrades to the pump stations. City also may want to explore joint efforts with the County to extend sewer from this site to the Route 52 pump station near Troy Howard Middle School, rather than relying upon the current sewer line which pumps sewage to the Wight Street area.
- 3) City should consider acquiring land adjacent to the Airport and Business Park to enhance operations of such.
- 4) Consistent with 3 above, City should consider the long-term need to expand the size of the current Business Park. The few remaining lots in the Park likely cannot serve a larger scale use.
- 5) All land adjacent to the Little River should be protected by a 250 foot deep Resource Protection Shoreland zone.

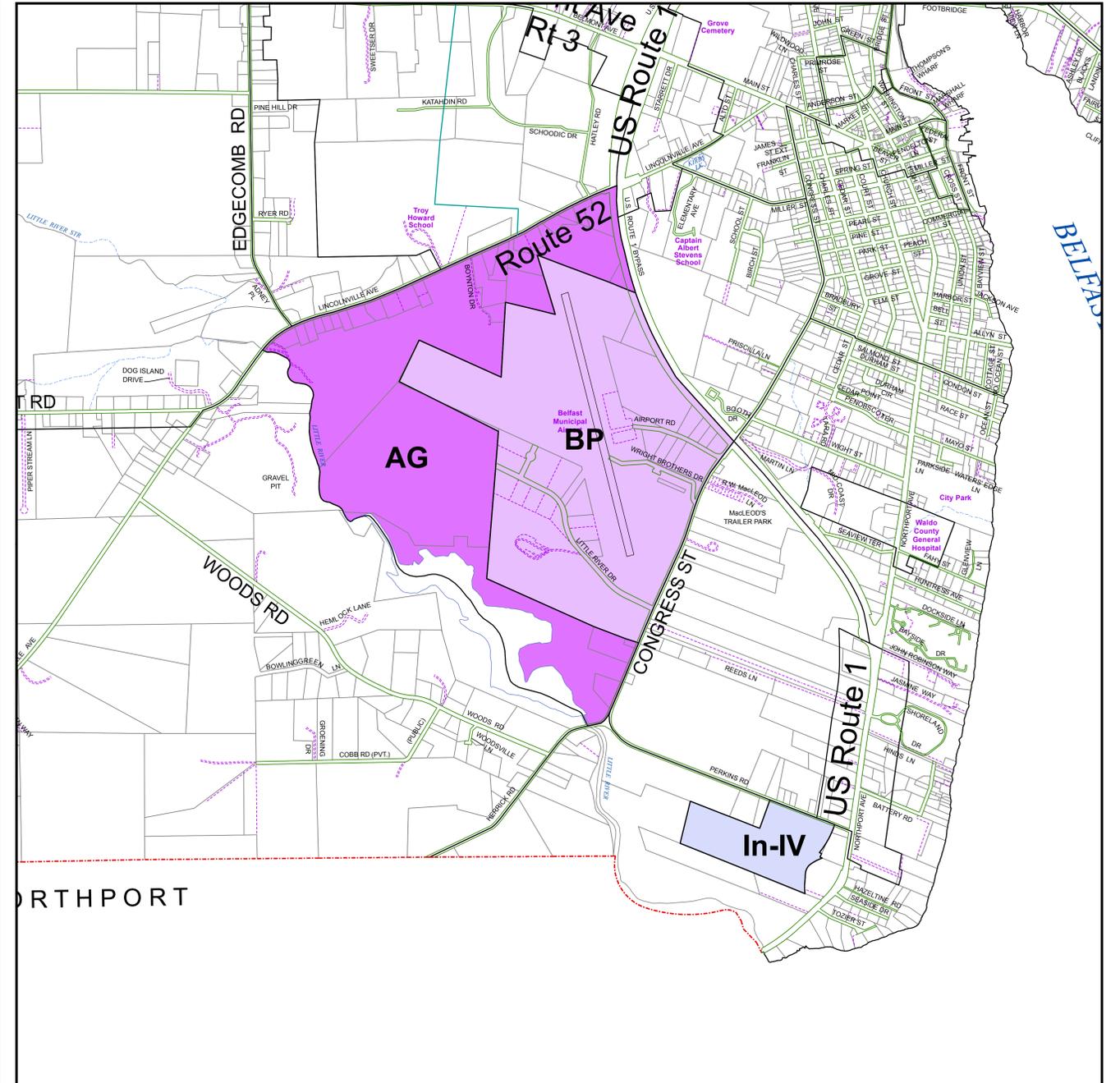
Business Park

PROPOSED LAND USE AREAS



Zone
BP Business Park

EXISTING ZONING



Zone
AG Airport Growth District
BP Business Park-Airport District
In-IV Industrial IV Perkins Road District

ROUTE ONE SOUTH MIXED USE

“SMALL SCALE COMMERCIAL & MIX”

Goal

To establish an area that recognizes the current mix of uses, retail stores, professional offices, service businesses, a hotel/restaurant, a light manufacturing business and residential, and to adopt regulations that allow such uses to co-exist while development of this area occurs.

Overview of Past, Current & Future Use

The ½ mile stretch of Route 1 located between Perkins Road to the south and the jug handle to the north has experienced a real mix of development. To the east of Route 1 lies Belfast Bay. The land area is about ¼ mile deep (between Rt 1 and the Bay) and the predominant development is residential. There are 3 main nonresidential uses located on the water side of the road, Moss, Inc. (manufacturer), State Farm Insurance (office), and Penobscot Bay Inn and Restaurant (hotel/restaurant), and all have frontage directly on Route 1. Most residences are closer to the coast, although some have frontage on Route 1, and one, Penobscot Shores, which is a retirement community, stretches from the road to the coast. The inland side of the road supports more business activity, with Aubochon Hardware the largest (about 20,000 sf) and the Irving Service Station the busiest. Several of the lots on the inland side of the area are quite large, but there is only a limited amount of remaining road frontage that is undeveloped.

Immediately to the north of the jug handle is a .75 mile stretch of Route One that fronts on the by-pass and which ends at Congress Street. The by-pass was constructed in the late 1960's and re-routed Route 1 so it no longer ran through the middle of in-town Belfast along Northport Avenue and High Street. The by-pass is a restricted access highway on which new curb-cuts/driveways are prohibited. The area to the east is part of Belfast's urban core. The area to the west is largely underdeveloped, although there are several uses located along outer Congress Street; an oil storage depot, a mobile home park, and several residences. This section of Congress Street also borders the City airport and Business Park.

Frankly stated, this area, unlike many in Belfast, has no central or overriding character. It is a mix of uses. The type and amount of commercial development appears to be limited by the area's location south of the Route 1 – Route 3 intersection. This section of Route 1 experiences much less traffic than near the Route 3 intersection or on the easterly side of the Memorial Bridge. There are several larger parcels near Congress Street, but to date, there has been little demand to develop such. Further, some of the soil conditions in the area are quite wet, which affects how much area can be developed. And, until late 2006, there was little public sewer in the area. Public sewer (gravity) has now been built

to the Perkins Road intersection, and there is some likelihood that development along Congress Street could tie into the City's force main line in this area by connecting to an existing pump station.

This is one of those areas in which the City should monitor future development activities to determine if one type of use becomes the most prevalent, and if it does, the City may want to reconsider the most appropriate zoning designation for this area. This is a 'Growth Area', mostly because of the general availability of public services (water & sewer), the area's location along or near a major highway (Route 1), and the amount of development which has occurred to date. There are additional opportunities for more development.

Recommendations

Permitted Uses (Examples of Main Uses)

- 1) Retail Stores, up to 20,000 square feet, except that in the outer Congress Street area, particularly properties that front on Congress Street, only smaller sized retail stores should be permitted.
- 2) Professional Offices
- 3) Service businesses
- 4) Auto sales, service & repair
- 5) Manufacturing/Light Industry
- 6) Hotel
- 7) Restaurant, however, fast food restaurants are prohibited
- 8) Warehouses & Storage, including fuel storage
- 9) Single family & two family residential, including manufactured housing
- 10) Home Occupations, Larger Scale

Minimum Lot Size

- 1) 1 acre (sewer or septic), residential or nonresidential
- 2) Minimum 150 ft of street frontage, with increase in amount of road frontage for nonresidential uses based on size of development/building or amount of traffic generated

Density (Number of Housing Units per Acre)

- 1) 1 single family or two family residential unit per acre
- 2) No specific restriction on amount of lot coverage for either a residential or a nonresidential use

Setbacks (Distance Structure must be Located from a Lot Line)

- 1) Residential Uses; Front setback of 30 feet and a side and rear setback of 15 feet.
- 2) Nonresidential uses; minimum 30 ft front setback, with increases based on size of use
- 3) Nonresidential uses; minimum 15 feet side and rear setback with increases based on size of development or proximity to existing residences

Major Changes Compared to Current Requirements

- 1) A major change is to extend the boundaries of the Route One South area to include most properties that are now located in the General Purpose-B zone. This includes properties that have street frontage on the northwesterly side of outer Congress Street. It is noted that the General Purpose-B zone allows a wide range/mix of uses, and that the proposed Route One South area would similarly allow a mix of uses.
- 2) A second major change is to decrease the type of housing and density of housing now permitted in either the current Route One South zone or the General Purpose B zone. Both zones currently allow multi-family housing and densities as great as 6 multi-family units per acre. The main reasons to decrease density are potential conflicts with future use and development of Belfast Municipal Airport and to emphasize that this area likely is better suited for nonresidential development in the long-term.
- 3) A third change is to revise the number of properties located on the easterly (water side) of the area that would be included in the Rt 1 South Mixed Use area. At present, all properties on the water side that are within 500 feet of Route 1 are in this area. This proposal would result in eliminating the Penobscot Shores property from this area, and the depth of the area would be determined by current property lines in the area. The intent is to include only properties that have road frontage on Route 1 in this area.

Other Issues & Ideas To Consider

- 1) It is important to manage conflicts with future use of airport by establishing the Airport Protection Overlay zone. This overlay district likely would have the greatest impact on properties located in the Congress Street area. The Airport Committee should assist in identifying potential conflict areas associated with current and future use of the airport.
- 2) There is a large amount of wetlands that affect the area located between Route One and Congress Street. This could adversely affect future development in this area.
- 3) This section of Route One is located within the urban compact zone, which means the City, and not MDOT, maintains the road and controls most decisions regarding access to Route 1.
- 4) MDOT, in 2007 – 2008, made initial improvements to the Congress Street and Route 1 intersection. In the longer term, this intersection likely warrants

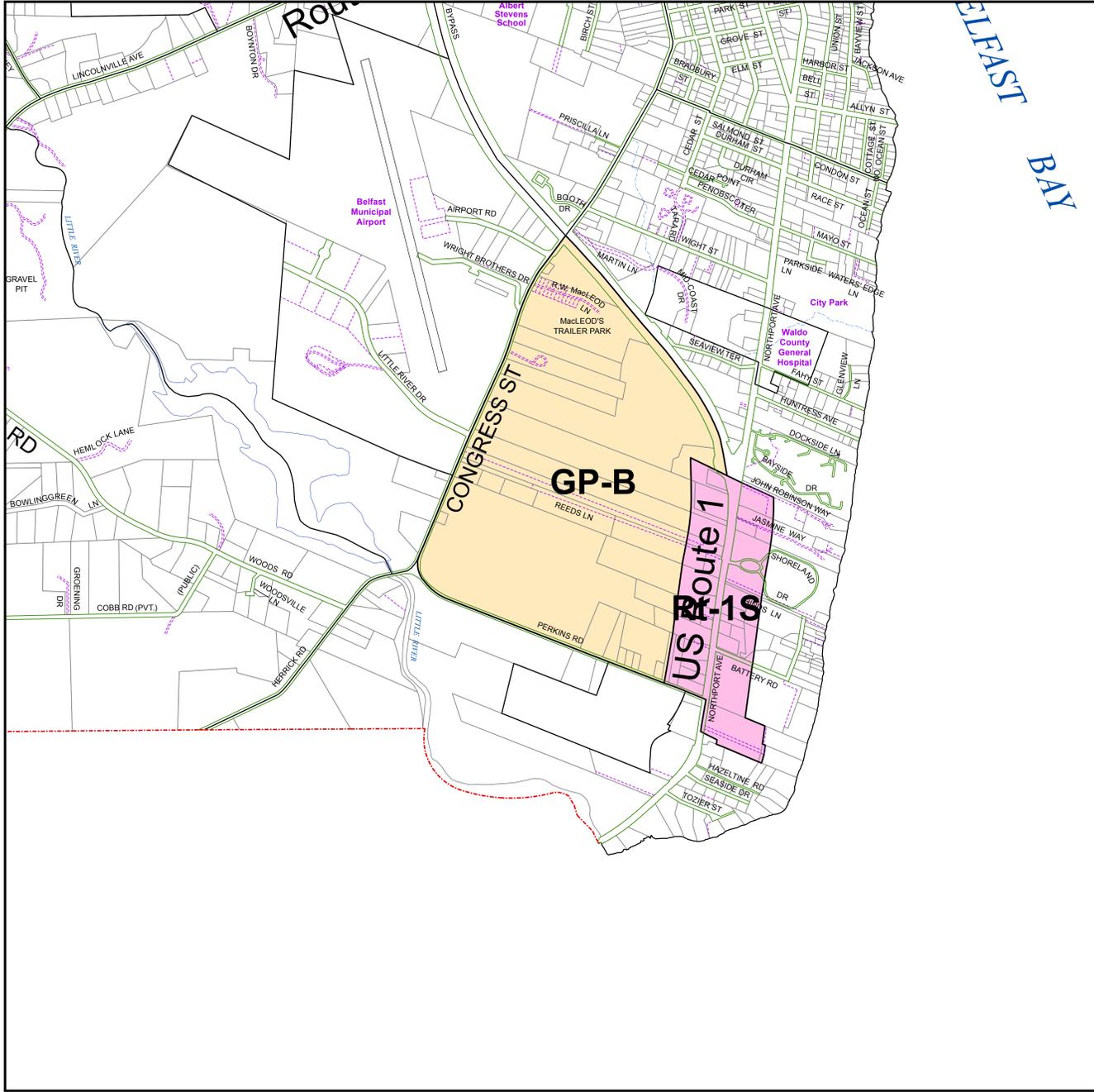
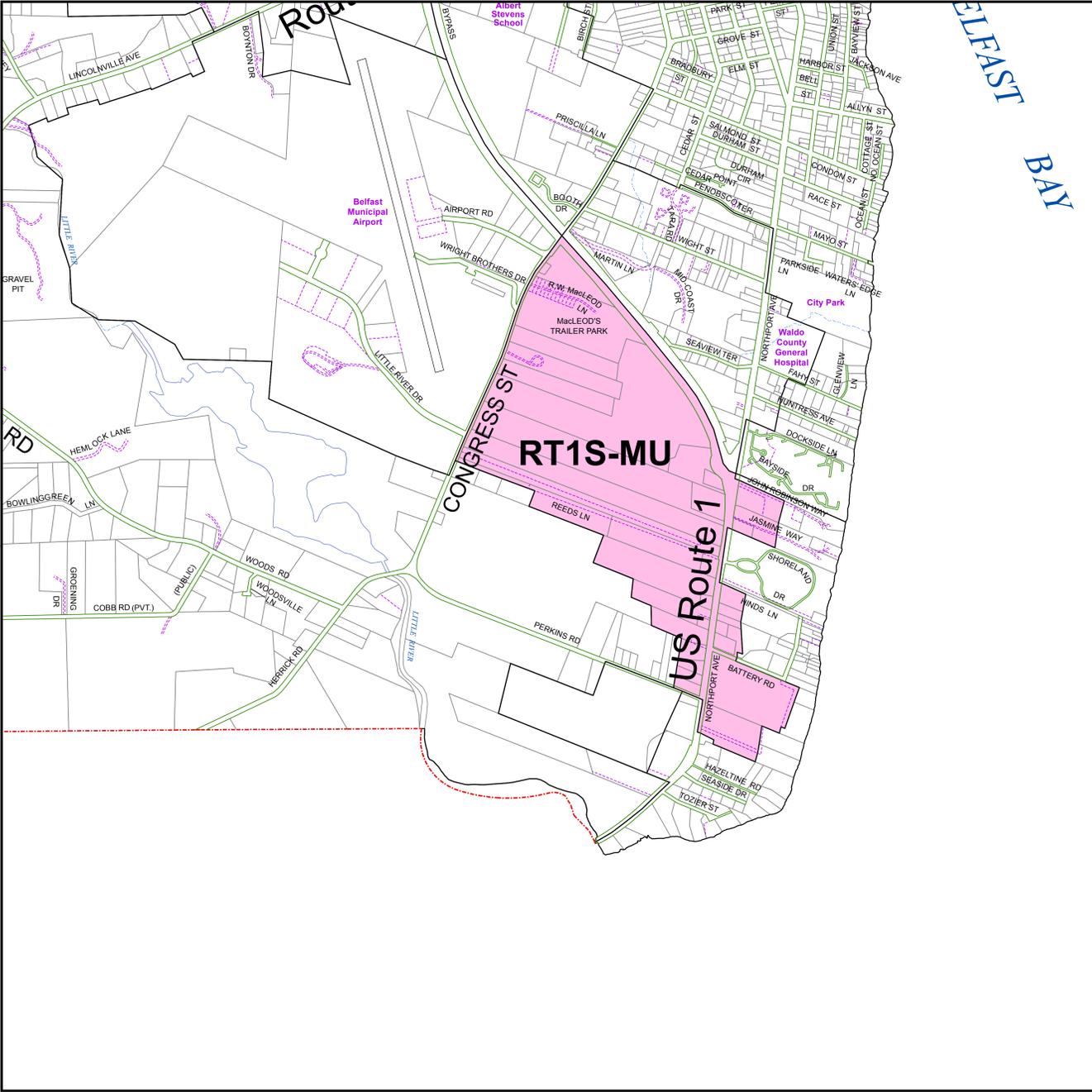
installation of a signal light, particularly if greater development occurs at the Business Park.

- 5) The limited access section of Route 1 located northerly of the jug handle often appears to be a rural area, partly because there are no curb-cuts and much of the area is wooded. The City may want to consider enacting regulations to require a protective wooden buffer strip along this section of Route 1 so the area retains its rural (forested) character.
- 6) The limited access highway requirement associated with the by-pass appears to have served Belfast well in helping to manage traffic in this area. The City should support the continuation of the limited access provision.

Route 1 South – Mixed Use

PROPOSED LAND USE AREA

EXISTING ZONING



Zone
 RT1S-MU Route 1 South – Mixed Use

Zone
 GP-B General Purpose "B" District
 Rt-1S Route One South Commercial District

RESIDENTIAL 6 AREA

“WATERFRONT HOUSING AREA”

Goal

To establish an area in which residential development along the outer section of Belfast Bay is the predominant use.

Overview of Past, Current & Future Use

Most of the area located along the Belfast Bay waterfront, from the jug handle on Route 1 to the Northport – Belfast line, has been developed for single family housing, and quite a bit of such has occurred on smaller lots. Many of the houses are built immediately adjacent to the coast or have views of the coast. The streets in the area usually run from Route One to the coast, and all end in a cul-de-sac or private turnaround area; there are no cross streets that connect these streets to one another. There are one or two larger undeveloped parcels, and the only nonresidential uses in the area are located immediately adjacent to Route 1. The amount of land area between the Bay and Route One is usually about ¼ mile in depth.

At present, little of the existing development in this area is served by public sewer, and most of the existing houses use individual septic systems. Public sewer ends at Perkins Road, and many of the residences are located south of this area along Tozier Street, Seaside Drive and Hazeltine Road. Also, many of the homes located northerly of Perkins Road are often near the coast, such as along Battery Road, and sewer to this area is via a private force main (pump line) and serves only a limited number of homes. While there are some opportunities for additional housing, these opportunities are quite limited. Also, all areas within 250 feet of the Bay are subject to City/State Shoreland requirements, usually requirements of the Limited Residential subdistrict. The exception is the Battery which is in the General Development Subdistrict, which is a shoreland zoning classification which the City should re-examine.

The Residential 6 zone is a ‘Growth Area’, but the total amount of new housing which can be constructed in this area will be limited. Also, most of the housing will be upper end homes because of the waterfront location or water views.

Recommendations

Permitted Uses (Examples of Main Use)

- 1) Single-family & two-family residences
- 2) Congregate Care & Elderly housing
- 3) Home Occupations, Small Scale (low impact)
- 4) Schools, Churches, Child Care & Similar Uses

Minimum Lot Size

- 1) 1/2 acre – 20,000 sq ft for single family if on sewer or septic
- 2) 1/2 acre – 20,000 sq ft for two-family if on sewer
- 3) 1 acre for two-family if on septic
- 4) 1 acre for nonresidential development
- 5) 100 feet road frontage, increasing to 200 feet if on Route One

Density (Number of Housing Units per Acre)

- 1) 2 single family houses per acre (sewer or septic)
- 2) 4 two-family units per acre if on sewer
- 3) 2 two-family units per acre if on septic
- 4) 0 multifamily housing per acre, it is a prohibited use
- 5) There is no specific density standard for congregate housing or elderly housing. Penobscot Shores is a congregate housing project.
- 6) It is noted that density for housing usually increases to 1 house per 30,000 square feet in the Shoreland Zone, however, it may be appropriate to allow a higher density standard (more units per acre) for select properties located in the Battery that are also in the Shoreland Zone.

Setbacks (Distance Structure must be Located from a Lot Line)

- 1) Residential uses; usually 30 feet front and 15 feet side & rear
- 2) Congregate Care & Elderly Housing, likely 30 feet front and 25 feet side & rear
- 3) Nonresidential uses, 30 feet front and 25 feet side and rear
- 4) No specific restriction on amount of lot coverage for either a residential or nonresidential use; unless property is located in the Shoreland area.

Major Changes Compared to Current Requirements

- 1) The Residential 6 area proposal would create a new zoning district which would replace the current Residential II zone that applied to most of this area. This change results in the following:
 - an increase in the minimum lot size from 15,000 square feet to 20,000 square feet, which reflects the unavailability of sewer to most of this area
 - an increase in the amount of road frontage from 60 feet to at least 100 feet and to 200 feet if the property is on Route 1. One hundred (100') is more appropriate for a 1/2 acre lot, however, small frontage lots should be prohibited on Route 1.
 - Eliminates offices uses which are now a permitted use in the Residential II zone.
- 2) Removes the section of the Penobscot Shores property that is located adjacent to Route 1 from the current Rte 1 South zone, which results in all of this property being included in one district. Also, several smaller properties that have no frontage on Route 1 would now be included entirely within a residential district. Properties located between the jug handle and Perkins Road that have frontage on

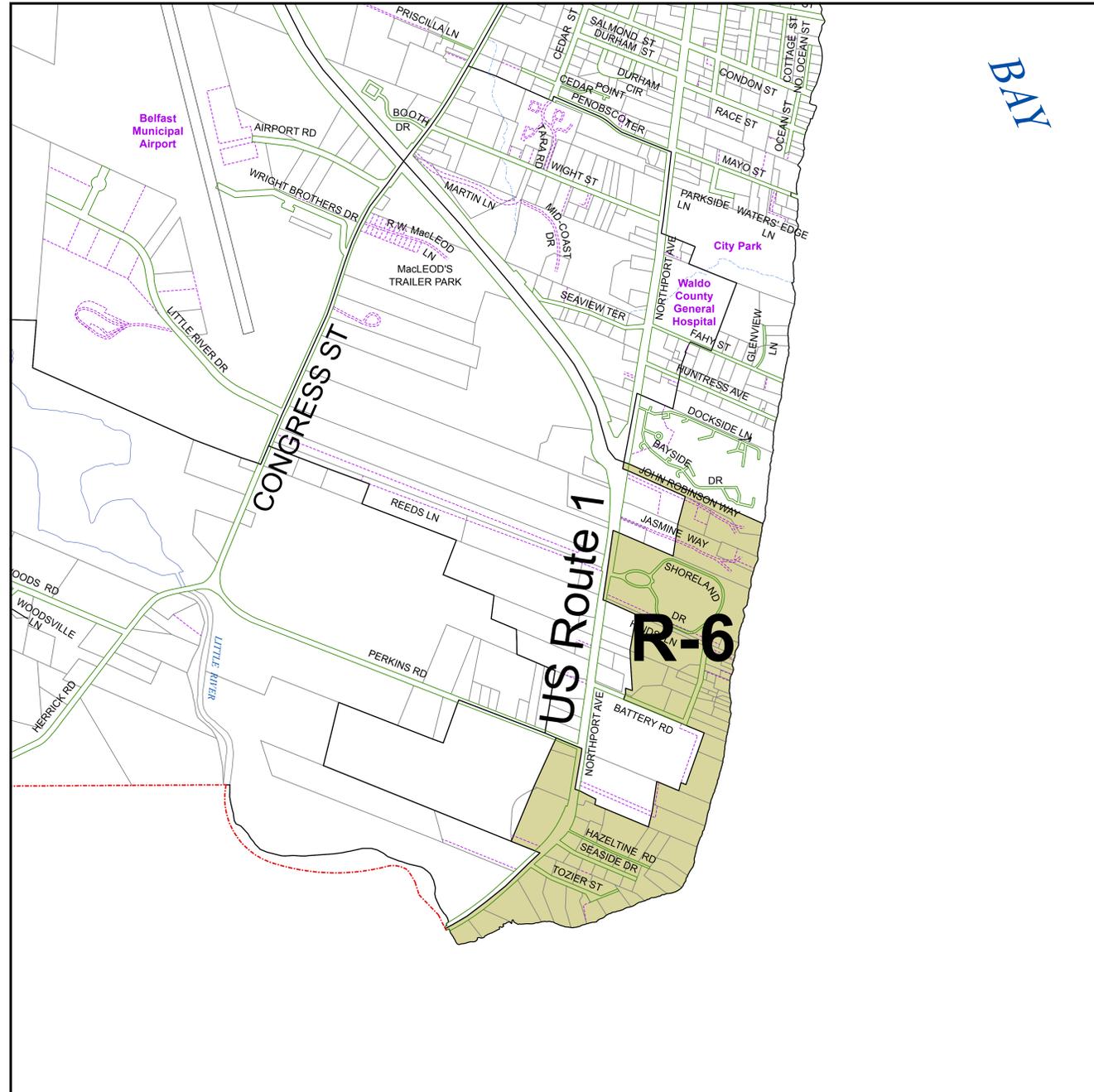
Route 1 would be in a mixed use district rather than this residential area, with the exception of the Penobscot Shores property.

Other Issues & Ideas

- 1) Much of the area is subject to Shoreland zoning requirements, which mandates setbacks from the water and increases in the minimum lot size. Area also includes the mouth of the Little River. Most of the Shoreland area is in the Limited Residential subdistrict, however the 'Battery' is now in the General Development subdistrict. The General Development subdistrict allows a wide range of uses, including most nonresidential uses. The City should consider amending the Shoreland classification for the Battery area.
- 2) The public sewer extension the City constructed on Route 1 to Perkins Road could eventually be used to extend sewer to residences located in the Hazeltine Road, Seaside Drive, and Tozier Street area, however, there appears to be little need to extend such service at this time. It is recommended that any such extensions occur at property owner expense rather than City expense, since most of the lots in this area have sufficient size to support on-site wastewater disposal, most of the available land has been developed, and there are limited opportunities for additional development.
- 3) Many of the homes in this area have high property values because of the waterfront location. Within the last five years, there has been growing interest in the construction of private docks. The City should monitor the amount of dock construction and where it occurs, and should consider amending its current dock regulations if too many docks are proposed. This concern would need to be examined by the Harbor Committee.

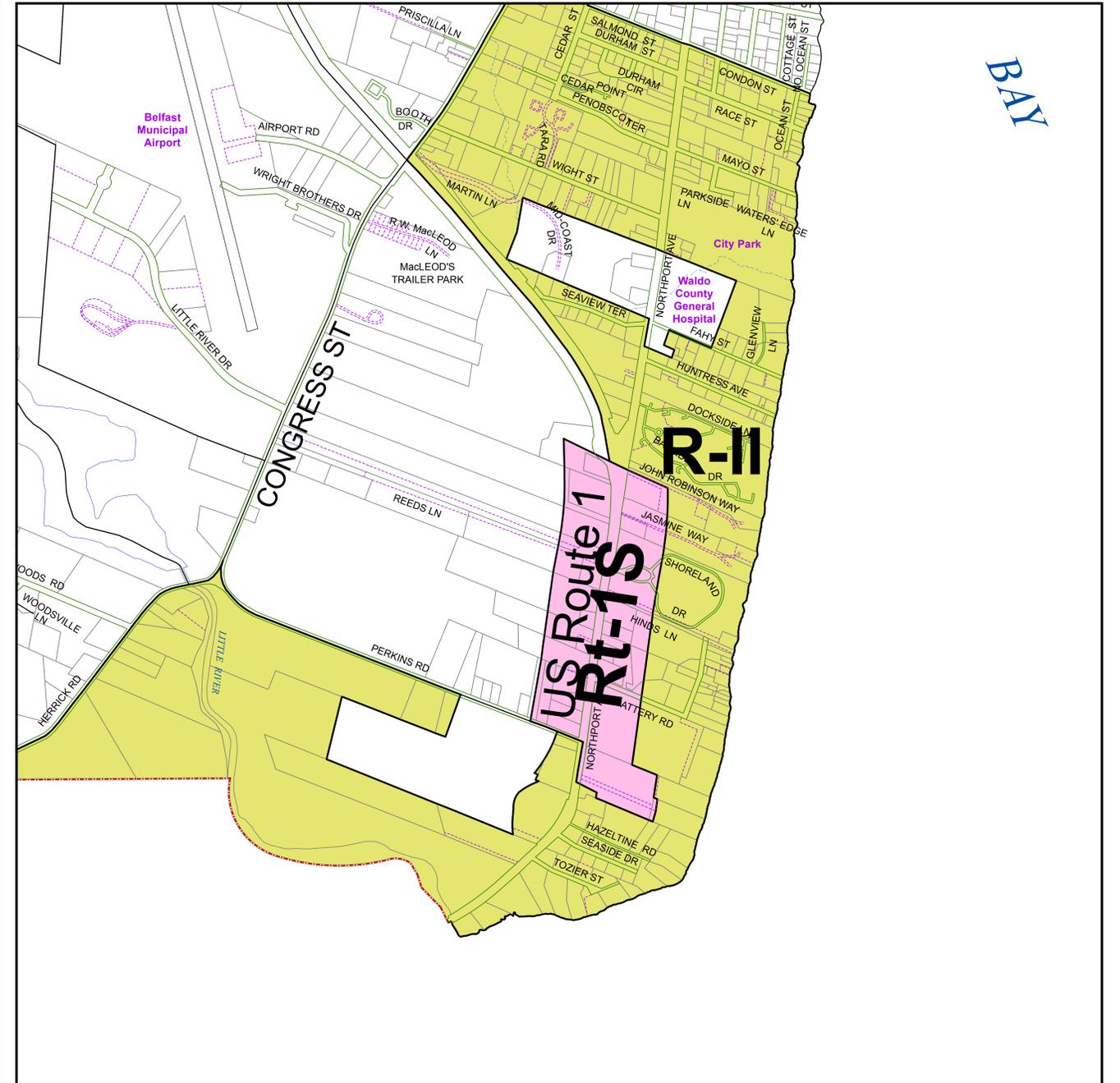
Residential-6

PROPOSED LAND USE AREA



Zone
R-6 Residential 6

EXISTING ZONING



Zone
R-II Residential II District
Rt-1S Route One South Commercial District